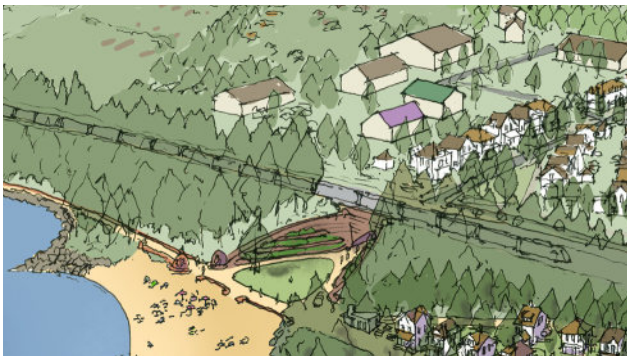

PONDERAY, IDAHO

ZONING CODE

CLIENT

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SPECIAL THANKS TO ALL MEMBERS OF THE PUBLIC WHO PARTICIPATED IN THE DEVELOPMENT OF THIS CODE.

TABLE OF CONTENTS

SECTION 1: DISTRICT STANDARDS

SECTION 2: BUILDING STANDARDS

SECTION 3: STREET STANDARDS

Section 1:
District Standards

APPROVAL PROCESS

This document contains regulations regarding the design of new buildings and public spaces. The purpose of this Code is to provide a common sense approach to building neighborhoods and open spaces consistent with the Sub-area Master Plan.

APPLICATION AND REVIEW

This Code is designed to be easy to read and implement. Zoning regulations have been reduced to the essential minimum standards. The Code provides wide latitude in the design of new development, and provides for expedited zoning approval for non-standard, or temporary, structures.

To seek zoning approval under this Code, submit a completed application to the Planning Director, accompanied by a current survey and any schematic drawings to prove complicity with the Code. Copies of the application can be requested from the Planning Director. Once submitted, and upon reviewing and validating that the application materials are complete and in compliance with this Code, the Planning Director shall issue an approval to the applicant. In the event the Planning Director determines that the materials are not complete or not in compliance, the Planning Director shall issue a determination of noncompliance to the applicant. In response, an applicant may submit supplemental or revised application materials, or submit a request for hearing before the Planning Board.

The Planning Director shall have the authority and flexibility to interpret the Code as appropriate. Applications consistent with the provisions of this Code shall be considered vest As-of-Right. After receiving approval from the Planning Director, this site plan may be filed at the City for approval at the same time as filing for preliminary plats with the County and in anticipation of construction permits.

PERMITTING PROCESS

Permits are required for two types of projects: Single Lot or Multi Lot. The following documents must accompany the application:

1. Survey, not older than two years.

2. An Illustrative Master Plan (Multi Lot)* or Site Plan (Single Lot) showing the project.

3. Any Diagrams, plans, sections, and elevations showing proposed District designation(s), Building Types, and Street Sections, and compliance with the Code.

***Illustrative Master Plan:** For applications over one lot in size, the application must show compliance with the Sub-area Master Plan. Compliance is measured through the provisions of this Code, and the connections and alignments codified in the Sub-area Master Plan (Figure 4). The Permit Application for a Multi Lot project shall include an Illustrative Master Plan, which shall delineate streets, alleys, and individual lots.

APPLICABILITY AND CONFLICTS

The provisions of this Code shall be enforced by the Planning Director. Provisions of the Code are activated by "shall" when required, "should" when recommended, and "may" when optional. Exceptions to these standards may be granted administratively by the Planning Director.

The Zoning Ordinances of the City of Ponderay shall continue to be applicable to issues not covered by this Code. In case of contradiction with local ordinances and/or safety codes, the Code shall be adjusted in coordination withe the Planning Director. This Code contains subdivision regulations, street cross sections for new streets, and building massing and placement standards. To the extent that any conflicts occur between the standards of this Code and the City of Ponderay Land Development Regulations, the provisions of this document shall prevail.

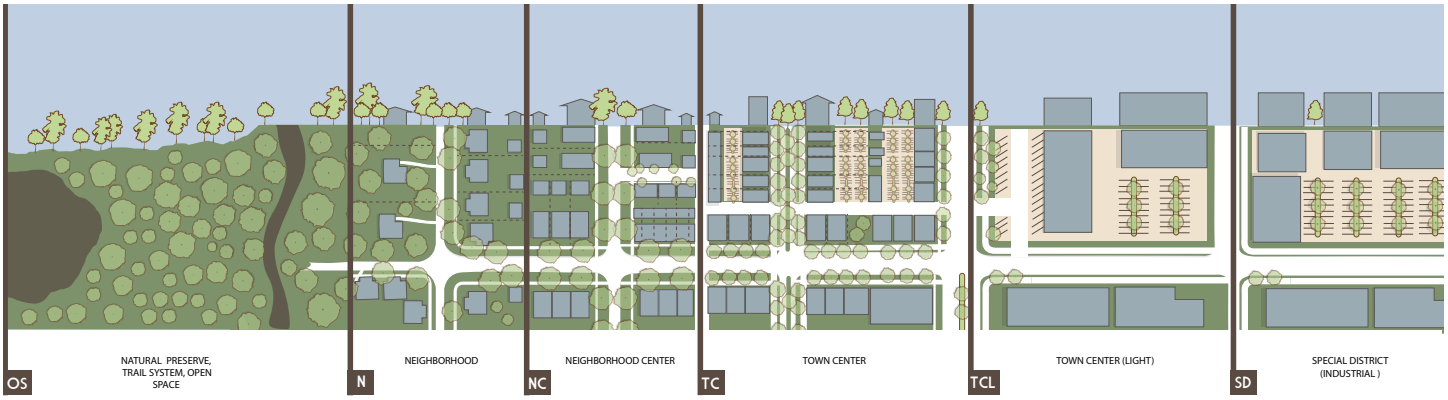

Figure 1: District Transects.

PURPOSE AND INTENT

The purpose of these regulations is to provide a framework for development that is both easy for citizens to implement and City staff to approve. The District Standards included herein are intended to be the minimal standards required to preserve public health and safety, and to provide for an enriching public realm. The Standards provide information at both the building scale (including Lot Area, Setbacks, and Building Frontage), and the block scale (Block Length, Intersection Density, and Block Perimeter). All developed lots within the Ponderay Sub-area Plan neighborhoods shall be allocated to one of the six Districts described in this section.

For lots within the Optional Overlay District areas (see Figure 2), the regulations of this Code are not required.

Figure 2: District Map.

Figure 3: Ponderay Landmarks.

1. City Hall
2. Pend D'Oreille Trail
3. Rail road
4. Highway 200
5. Idaho Transportation Department
6. Harbison Farm
7. Anderson's Autobody, Inc
8. Auto Alley

Figure 4: Illustrative Master Plan.

The Main Goals of the Sub-area Illustrative Master Plan and Code are:

- Enhance connectivity between Ponderay and its lake front.
- Create a framework for the development of safe streets that works for all users.
- Support incremental, high-return economic and physical growth and sustainable economic development.
- Increase overall quality of life for Ponderay residents.
- Integrate an incremental and lightweight development approach, giving careful consideration to opportunities for short-term action that can start advancing the community's 30-year vision in the near-term.

DISTRICT STANDARDS: OVERVIEW

UNDERSTANDING THE DISTRICT STANDARDS

In the following pages, District regulations at the lot, building, and block scale are presented. Adjacent is an example of the table found on each District page with information to guide future development, as well as an explanatory diagram of the figures found on each page.

DEFINITIONS

Active Frontage specifies the amount of the building frontage that must be "active", meaning that the frontage enables visual engagement between passersby on the street, and people occupying the ground floor of the building. This is typically accomplished by continuous shopfronts, building entrances, windows, and open facades.

As-Of-Right gives something a legal claim or entitlement.

Block Length refers to the recommended longest distance between intersections or streets in an area.

Block Perimeter measures the total linear distance of the perimeter of a block.

Building Frontage are those types of conventional building fronts (see Pages 28-29) suggested in the District. Building fronts shall be allowed to encroach into the Setback area.

Building Height is calculated in number of stories. One story is up to 14 feet high.

Building Facade is the exterior face of a building that is oriented toward the Principal Front.

Building Types are the categories of buildings specified in Section 2, Building Standards of this Code.

Illustrative Master Plan delineates streets, alleys, lots, and the location of proposed development.

Intersection Density is a measure of the connectedness of a street network. It is the number of intersections/square mile, where an "intersection" is the meeting of three or more publicly accessible streets. A minimum of 90 intersections/square mile is required of new development.

Lot Area refers to the maximum allowable square footage of the lot within the property lines.

Lot Width refers to the range allowed for the width of lots

Micro Residential is a residential structure with a footprint of less than 500 square feet (500 sf). It can be one or two stories, and may contain a garage. These structures are allowed As-Of-Right in any District set forth in this Code.

Micro Retail is any structure that has a footprint less than 500 square feet (sf), is used primarily for retail purposes, and is allowed As-Of-Right in any District set forth in this Code.

Multi-Lot Development is the development of multiple lots simultaneously.

Setbacks are the distances between the edge of a building and the property line, and are specified in the table for the front and rear of a building. The Principal Front refers to side that faces the street of higher importance. The Secondary Front refers to the side that faces the street with less importance.

Single Lot Development is the development of one lot.

A. GENERAL DESCRIPTION
This description is an overview of the District that specifies lot, building, and block regulations, and attributes that make the District unique from the others.
B. LOT
Area
Width
C. BUILDING
Active Frontage
Height
BUILDING FRONTAGE
SETBACKS
Principal Front
Secondary Front
Rear
D. BLOCK
Block Length
Intersection Density
Block Perimeter
E. ALLOWED BUILDING TYPES

Figure 5: Example District Standards Table.

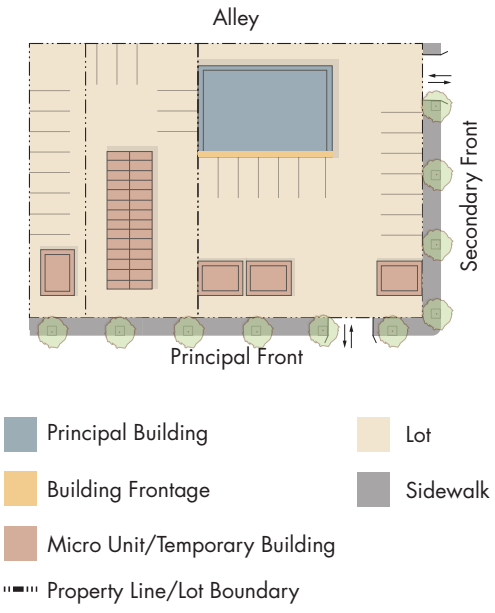

Figure 6: Example District diagram.

TRANSITIONAL APPROACH

This Code encourages micro units/temporary buildings (indicated in Figure 6) in order to expedite the creation of housing and commercial opportunities on built and vacant lots. The Building Types listed as "Micro" (Pages 25-26) are allowed As-Of-Right by this Code, without extra permitting required.

ZONE	TC	TCL	NC	N	SD	OS
RESIDENTIAL						
Mixed-Use Building	●	●	●	○	○	
Apartment Building	●	●	●			
House (up to 6 units)	○	○	●	●	○	
Assisted Living Facility	●	●	●	○	○	
LODGING						
Hotel (no room limit)	●	●	○			
Inn (up to 15 rooms)	●	●	●			
OFFICE						
Office Building	●	●	●		●	
Live-Work Space	●	●	●			
RETAIL						
ShopFront Building	●	●	●		○	
Restaurant	●	●	●		○	
Micro-Retail	●	●	●		●	
CIVIC						
Bus Shelter	●	●	●		●	
Fountain or Public Art	●	●	●	○	●	●
Library	●	●	●		○	
Theatre	●	●	●		●	
Museum	●	●	●		●	
Parking Structure	●	●			●	
Surface Parking		●	●		●	
Religious Assembly	●	●	●	○	○	
Playground	●	●	●	●	●	●
Recreational Field		●	●	○	○	●
Trail Head						●
OTHER: AUTO						
Gas Station		●		○	●	
Auto Service					●	
Drive- Through	●	●				
OTHER: CIVIC SUPPORT						
Fire Station	●	●			●	
Police Sub Station	●	●	○	○	○	○
Medical Clinic	●	●	○	○	●	○
OTHER: EDUCATION						
Childcare Center	●	●	●	●		
Elementary School	●	●	●	●	○	○
Middle School	●	●	●	●	○	○
High School	●	●	●	●	○	○

● Allowed (permanent)
○ Transitional

Figure 7: District Allowed Uses.

PARKING ACCESS AND UTILITIES

All lots in the Illustrative Plan have rear alleys, from which parking can be accessed. When access to parking behind Principal Buildings is present, driveways should be minimized along the Primary and Secondary Fronts. Parking should never be located in the Active Frontage area.

Utilities, such as electrical, plumbing, and mechanical equipment, shall not be placed in the Active Frontage area or within 20' of same along side streets.

ALLOWED USES

Adjacent is a table that outlines the allowed uses for each District, broken into Residential, Lodging, Office, Retail, Civic, Auto, Civic Support, and Education categories and subcategories. Note the difference between Transitional and Permanent allowed uses.

GENERAL CONDITIONS

When two Districts abut, the one with the more urban standards prevails, in the following descending order: Town Center, Town Center Light, Neighborhood Center, Neighborhood, Special District.

For all other regulations regarding lighting, landscaping, and signs, refer back to the City of Ponderay Zoning Code.

TOWN CENTER

A. GENERAL DESCRIPTION

Town Center contains buildings that meet the street at the property line and frontage, have awnings, galleries, or other appropriate forms of weather protection for pedestrians. Buildings are generally attached, providing a continuous frontage. This zone accommodates commercial, retail, office, and multi-family residential.

B. LOT

LOT OCCUPATION

Area	40,000 sf max
Width	25′ min - 120′ max

C. BUILDING

Active Frontage	70% min
Height	1 story min, Facade 14′ min

BUILDING FRONTAGE

Shopfront	Arcade	Gallery	Facade

SETBACKS

Principal Front	10′ max
Secondary Front	10′ max
Rear	10′ min

D. BLOCK

Block Length	450′ max
Intersection Density	100/sq mi
Block Perimeter	2,000′ max

E. ALLOWED BUILDING TYPES

Main Street Building	Apartment Building
Micro Retail	

F. NOTES

- 15′x20′ Micro Retail garages and 20′x60′ Quonset Hut used to transition from single-building and empty lot occupation to multiple Main Street Buildings and Townhouse with continuous frontage.
- Lots with existing buildings can accommodate additional retail space in the existing parking lots.
- Lots can be subdivided to separate and accommodate new buildings.
- Active Frontage requirement can be met using temporay uses.

Principal Building

Building Frontage

Micro Unit/Temporary Building

Property Line/Lot Boundary

Lot

Sidewalk

LONG - TERM CONDITION

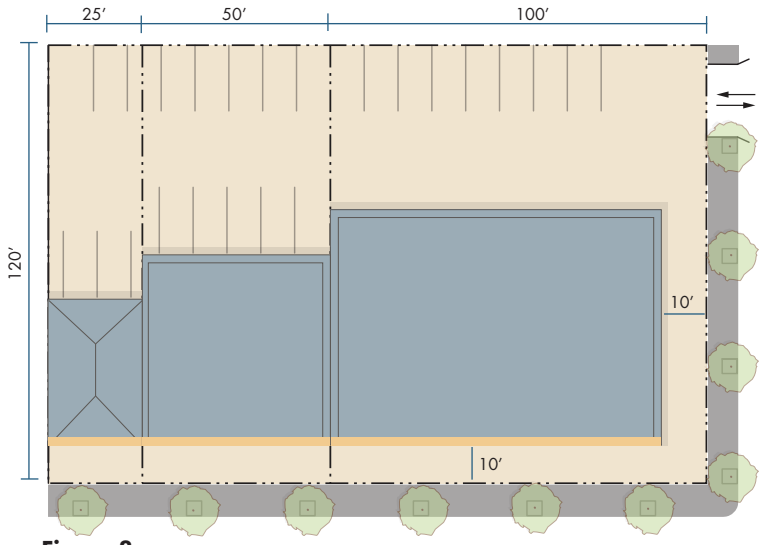

Figure 8

TRANSITIONAL CONDITION

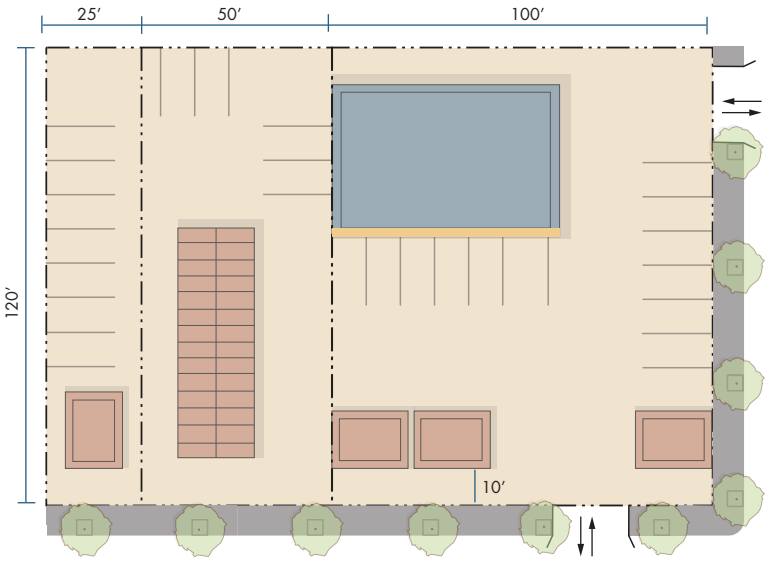

Figure 9

EXISTING CONDITION

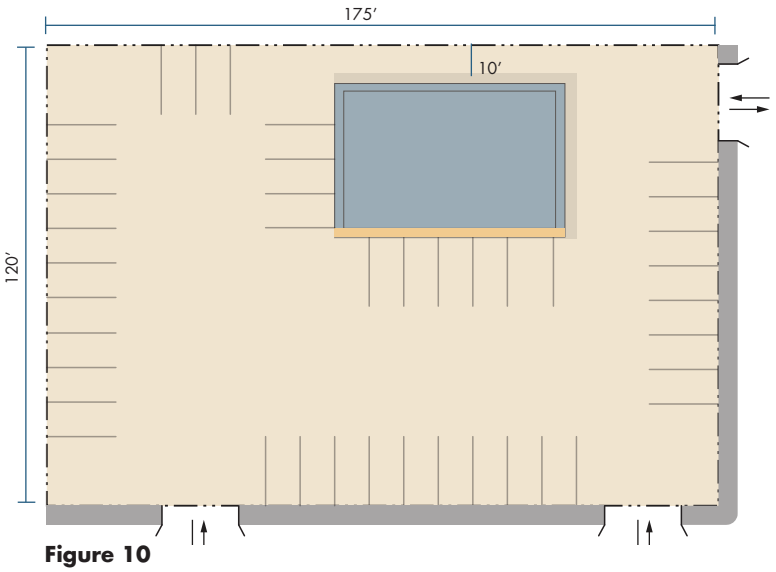

Figure 10

A. GENERAL DESCRIPTION

Town Center Light (TCL) contains buildings that meet the street at the property line, have awnings, or other appropriate forms of weather protection for pedestrians. This District accommodates commercial retail, offices, large storage and medical uses. In addition, Town Center Light also allows for parking in the primary and secondary layers through the creation of a secondary frontage road with the lot area.

B. LOT

Area	60,000 sf max
Width	25′ min - 120′ max

C. BUILDING

Active Frontage	80% min
Height	1 story min; Facade 14′ min

BUILDING FRONTAGE

Stoop		Shopfront
Arcade	Gallery	Facade

SETBACKS

Principal Front	35′ max
Secondary Front	12′ max
Rear	10′ min

D. BLOCK

Block Length	500′ max
Intersection Density	90/sq mi
Block Perimeter	2,000′ max

E. ALLOWED BUILDING TYPES

Main Street Building	Micro Retail
Industrial Building	

F. NOTES

- 15′x20′ Micro Retail garages used to fill gap between an existing Industrial Building and Main Street Building to accomplish continuous frontage.
- Lots with existing buildings can accommodate additional retail space in the existing parking lots.
- Vacant lots may temporarily be used as "pop-up" retail space. Reconfigure the existing parking to accommodate a slip lane with parallel and angled parking.
- Lots can be subdivided to separate and accommodate new buildings.

Principal Building
Building Frontage
Micro Unit/Temporary Building
Property Line/Lot Boundary
Lot
Sidewalk

LONG - TERM CONDITION

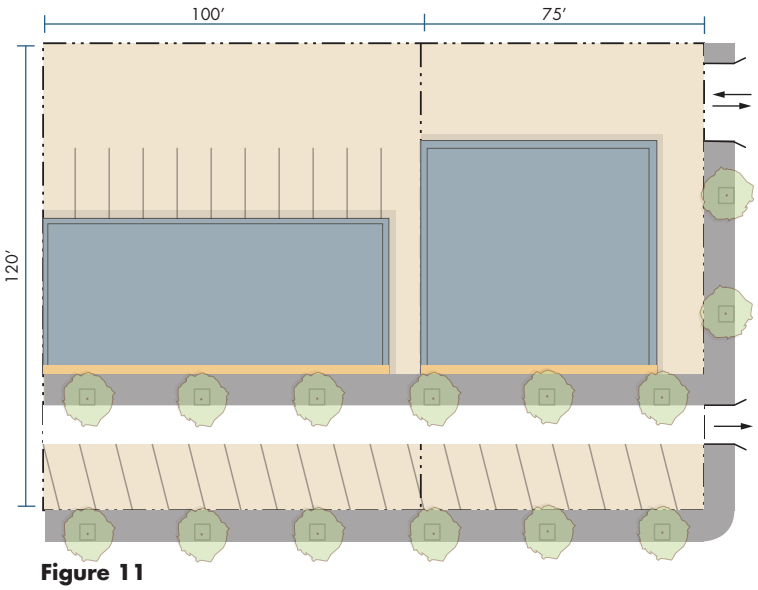

Figure 11

TRANSITIONAL CONDITION

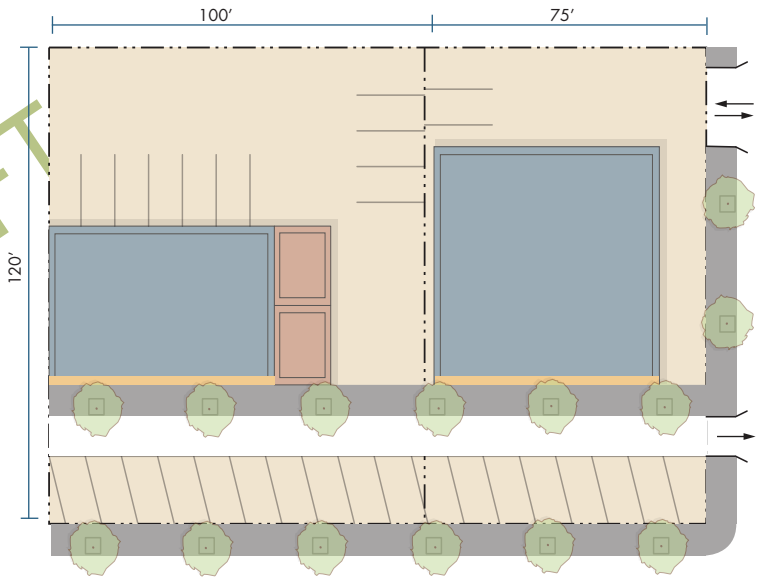

Figure 12

EXISTING CONDITION

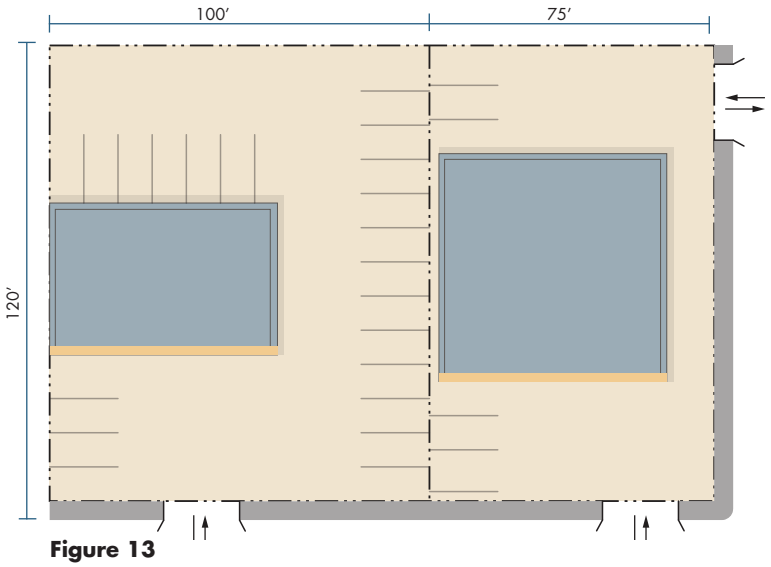

Figure 13

NEIGHBORHOOD CENTER

A. GENERAL DESCRIPTION

Neighborhood Center (NC) consists of mostly multifamily residential uses while accommodating small footprint retail and mixed-use. Setbacks and landscaping are variable.

B. LOT

Area	10,000 sf max
Width	18′ min - 120′ max

C. BUILDING

Active Frontage	60% min
Height	3 stories max

BUILDING FRONTAGE

Porch	Stoop		Shopfront
Arcade	Gallery	Shed Roof	Facade

SETBACKS

Principal Front	15′ max
Secondary Front	15′ max
Principal Rear	20′ min
Accessory Dwelling Unit Rear	0′

D. BLOCK

Block Length	450′ max
Intersection Density	100/sq mi
Block Perimeter	2,000′ max

E. ALLOWED BUILDING TYPES

Apartment Building	Cottage Court
Micro Residential	

F. NOTES

- 20′x20′ Micro Residential units are used to transition to a larger Apartment Building, and front and rear 8′ porches and 20′x20′ Accessory Dwelling Units are added onto existing Apartment Houses and Houses.
- Accessory Dwelling Unit frontage can face any direction.
- Lots with existing buildings can accommodate additional retail space in the existing parking lots.
- Lots can be subdivided to separate and accommodate new buildings.

LONG - TERM CONDITION

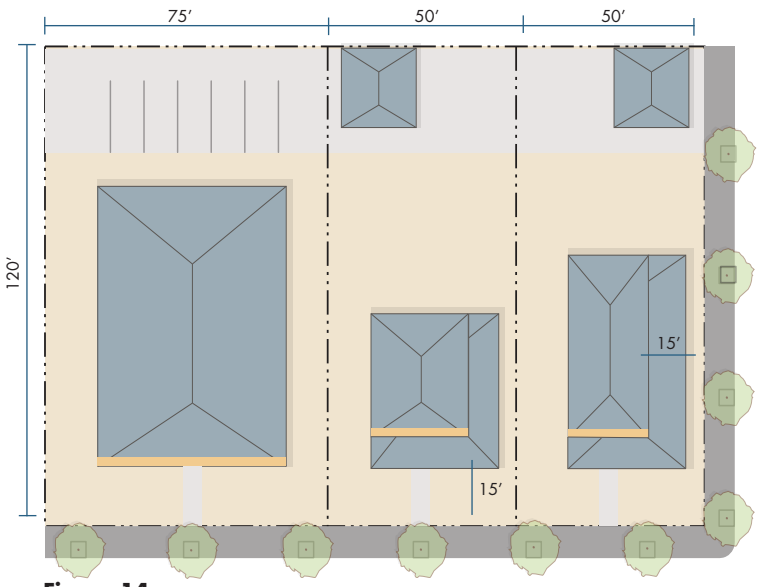

Figure 14

TRANSITIONAL CONDITION

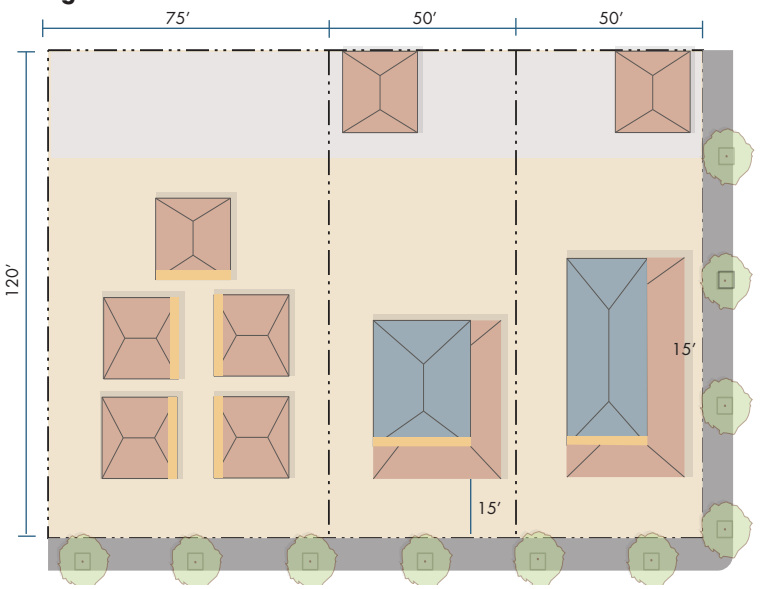

Figure 15

EXISTING CONDITION

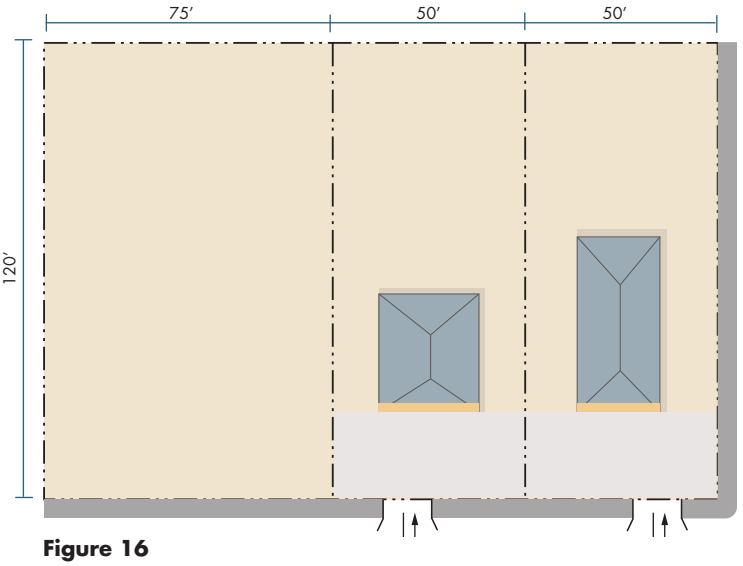

Figure 16

Principal Building
Building Frontage
Micro Unit/Temporary Building
Pavement
Property Line/Lot Boundary
Lot
Sidewalk

A. GENERAL DESCRIPTION

Neighborhood (N) is made up of single family homes and duplex/triplex units in a variety of building types. Parking is located at the rear of the lots and can be accessed through the alley or the front of the lot. Lots may be enclosed with a hedge or low fence, and setbacks are relatively deep.

B. LOT

Area	14,400 sf max
Width	18′ min - 120′ max

C. BUILDING

Active Frontage		N/A
Height		2 stories max
BUILDING FRONTAGE		
Common Yard	Porch	Shed Roof
SETBACKS		
Principal Front		24′ min
Secondary Front		12′ min
Side		5′ min
Principal Rear		25′ min
Accessory Dwelling Unit Rear		0′

D. BLOCK

Block Length	350′ max
Intersection Density	200/sq mi
Block Perimeter	1,500′ max

E. ALLOWED BUILDING TYPES

House	Micro Residential

F. NOTES

- 8′ porches are added onto existing Houses. A temporary trailer and 20′x20′ Micro Residential units are made permanent in the long-term condition.
- Accessory Dwelling Unit frontage can face any direction.
- Temporary trailer frontage changes from facing inward to facing Primary Front in long-term condition.
- Lots with existing buildings can accommodate additional retail space in the existing parking lots.
- Lots can be subdivided to separate and accommodate new buildings.

Principal Building
Building Frontage
Micro Unit/Temporary Building
Pavement
Property Line/Lot Boundary
Lot
Sidewalk

LONG - TERM CONDITION

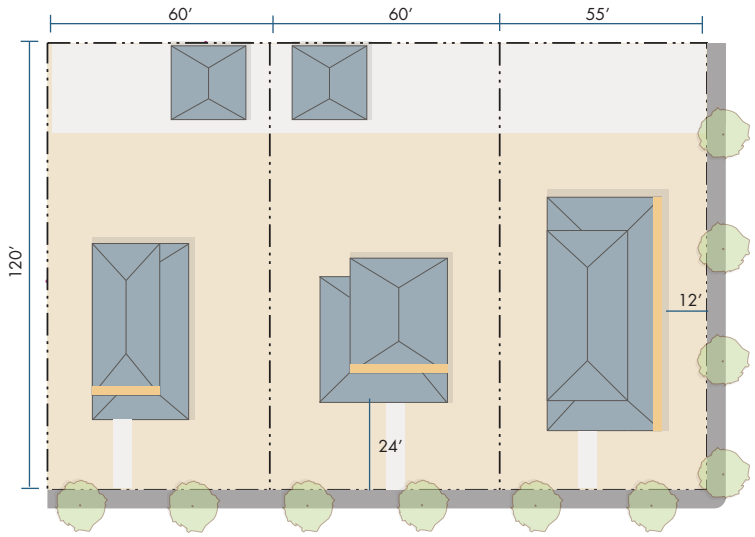

Figure 17

TRANSITIONAL CONDITION

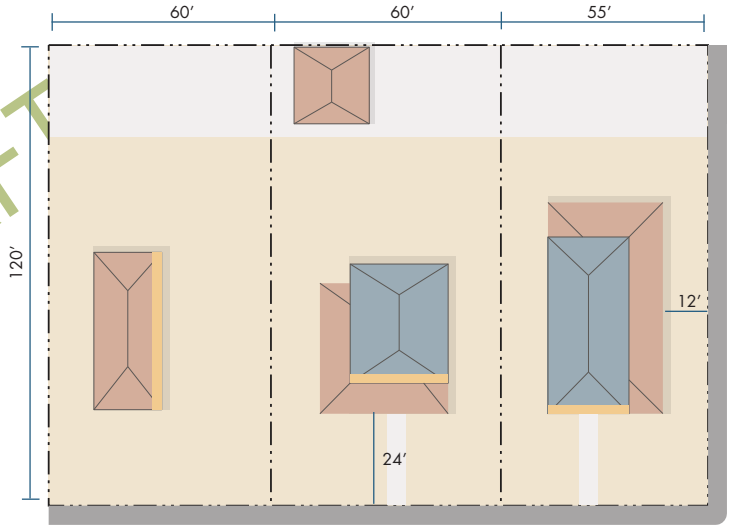

Figure 18

EXISTING CONDITION

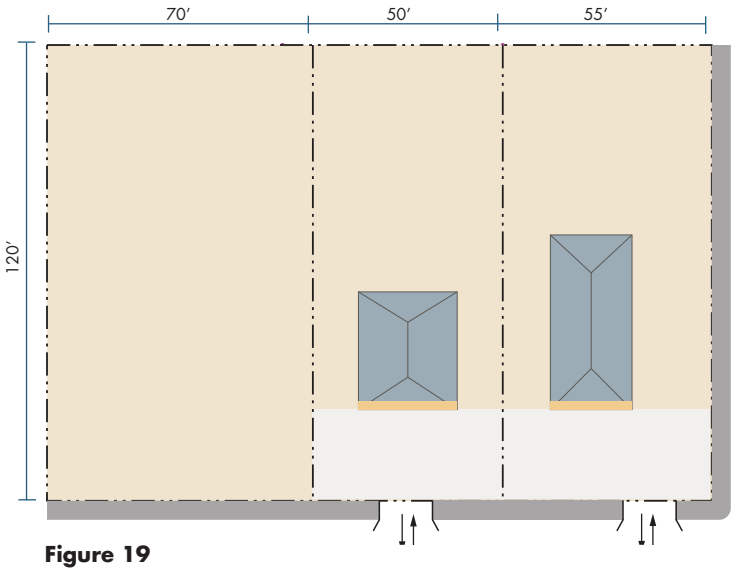

Figure 19

SPECIAL DISTRICT (INDUSTRIAL)

A. GENERAL DESCRIPTION

Special District (Industrial) is comprised of large footprint buildings. Parking is located in the rear of the buildings. Roads may be wider to accommodate greater turning radius of large vehicles.

B. LOT

Area	N/A
Width	N/A

C. BUILDING

Active Frontage	35%
Height	1 story min; Facade 14′ min
BUILDING FRONTAGE	
Arcade	Gallery
SETBACKS	
Principal Front	12′ max
Secondary Front	12′ max
Side	0′
Rear	10′

D. BLOCK

Block Length	450′ max
Intersection Density	100/sq mi
Block Perimeter	2000′ max

E. ALLOWED BUILDING TYPES

Industrial Building	Micro Retail

F. NOTES

- Micro Units/Temporary buildings can be placed anywhere on the lot.
- Quonset Huts are used to create Active Frontage on the Principal Front, and to transition to a Principal Industrial Building.

LONG - TERM CONDITION

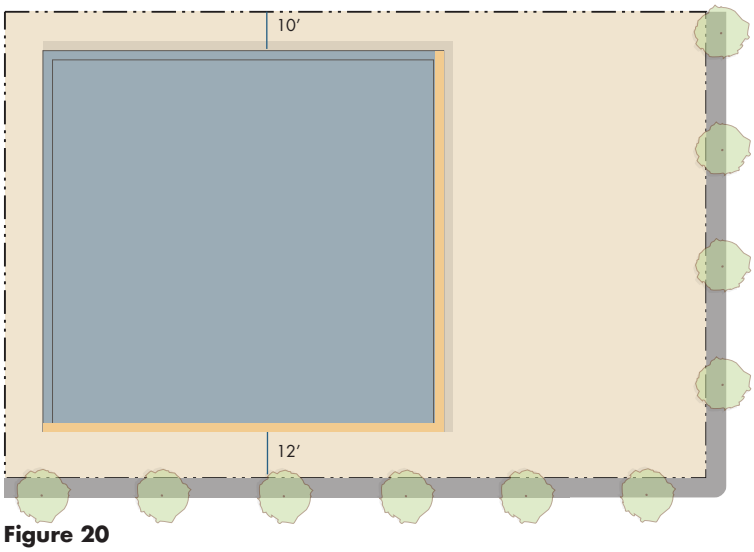

Figure 20

TRANSITIONAL CONDITION

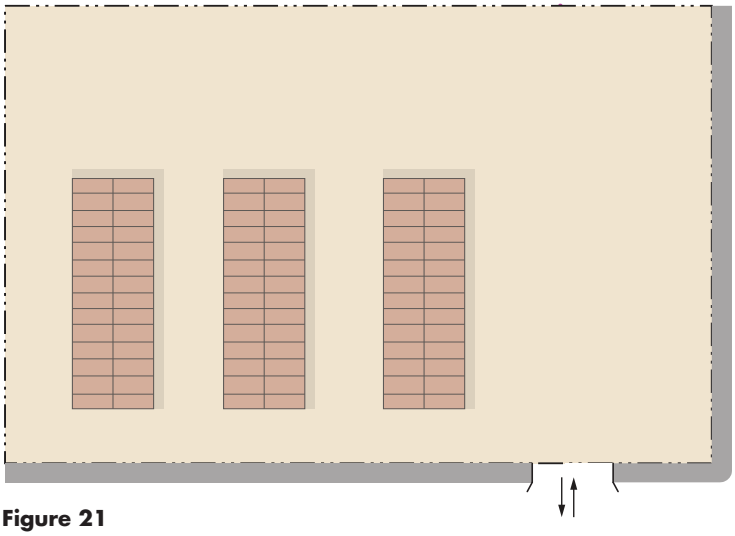

Figure 21

EXISTING CONDITION

Figure 22

Principal Building
Building Frontage
Micro Unit/Temporary Building
Property Line/Lot Boundary
Lot
Sidewalk

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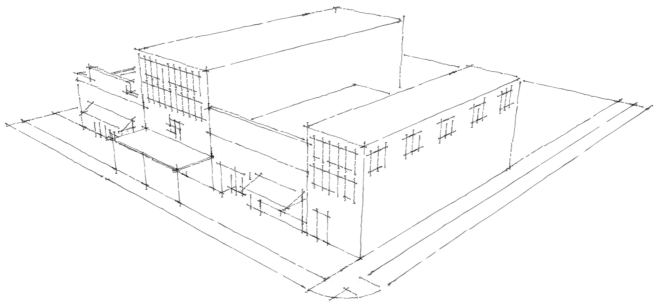
Section 2:
Building Standards

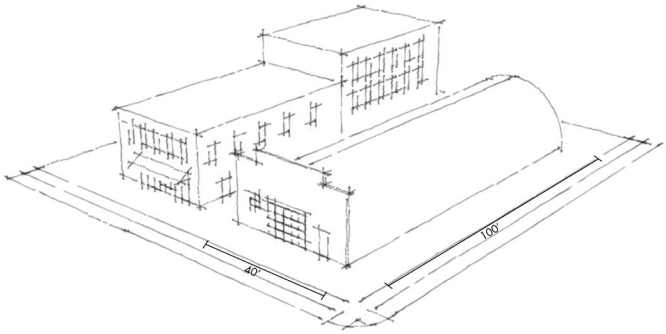
BUILDING TYPES SUMMARY

Section 2 of this code Building Standards provide criteria that builders and architects shall follow in designing structures within the Sub-area Master Plan. Structures and styles may deviate from those shown, provided they meet the dimensional criteria listed. Designs following these standards are granted As-Of-Right zoning status, requiring nothing more than a simple application showing complicity with the Code, a survey, and schematic design. Building Types are shown for each District, followed by Building Fronts and Transitional Building Fronts.

	TC	TCL	NC	N	SD
BUILDING TYPE					
Main Street	●	●			●
Multi-Story Mixed-Use					
Quonset Hut					
Apartment Building	●		●		
Townhouse	●		●		
Detached Apartment Building			●		
Apartment House			●	●	
Two-Story			●		
One-Story			●	●	
Cottage Court			●	●	
Single-Family House				●	
Micro Residential			●	●	
Primary Residential Unit					
Accessory Residential Unit					
Container Home					
Micro Retail	●	●	●		●
General Micro Retail					
Food Trucks					
Industrial Building					●
General Industrial Building					
Micro Industrial Building					
Civic Building	●	●	●	●	

Figure 23: Building Type Matrix.





MULTI-STORY MIXED-USE

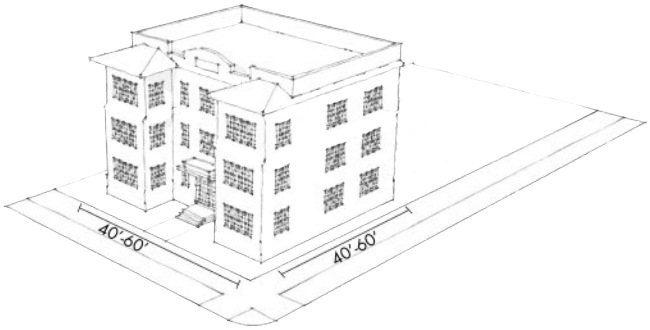
Mixed-Use Buildings features offices and retail stores at the first story, and either offices or lodging units above. This is the most common building type in TC. A zone with a large number of this type of building is walkable to the convenience of multiple uses within close proximity to each other. Due to the fact that the building may have multiple uses, the buildings will be occupied at different times of the day, providing natural surveillance on the street that the building faces. In order to successfully activate pedestrian life, these buildings shall provide an awning/marquee, gallery, or arcade that is continuous. See frontage types for additional information. A Main Street condition can be accomplished either with buildings at different heights, like one or two-story buildings, or with buildings all at the same height .

QUONSET HUT

A Quonset Hut is a prefabricated structure comprised of corrugated steel, with a semicircular cross section. These structures are traditionally used as storage facilities, and can be adapted for commercial and/or retail use. The Facade of a Quonset Hut can be retrofitted to fit the character of the other buildings around it.

This building type can house temporary uses, like on vacant lots, or function as standalone permanent structures in a Main Street or Downtown condition.

APARTMENT BUILDING



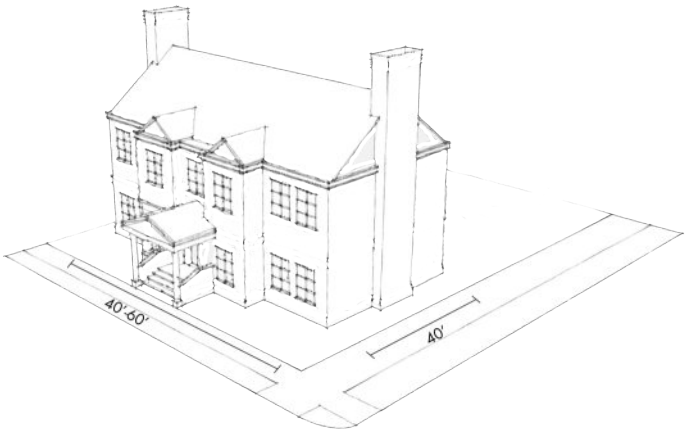
TOWNHOUSE

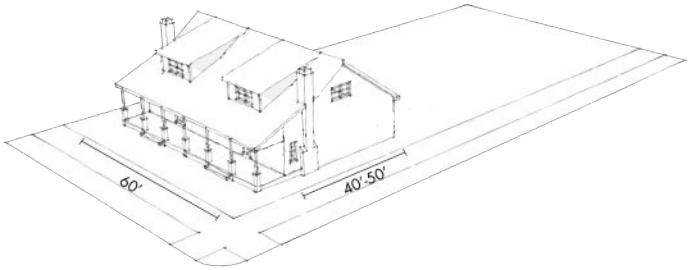
Townhouses are attached single family structures that share a common fire-rated wall with an adjacent unit of the same type. This housing type is a common way to provide density and housing type diversity while still providing affordable housing at a human scale. The typical height is two floors. Townhouses are rear-loaded for parking access, and are therefore only allowed if a functional alley is in place.

DETACHED APARTMENT BUILDING

This building type has a medium to large size footprint, made up of 4-8 stacked or side by side dwelling units with a shared entry. The Detached Apartment Building type is appropriately scaled to fit within medium density neighborhoods, or sparingly within large-lot and predominantly single-residence neighborhoods.

APARTMENT HOUSE





TWO-STORY

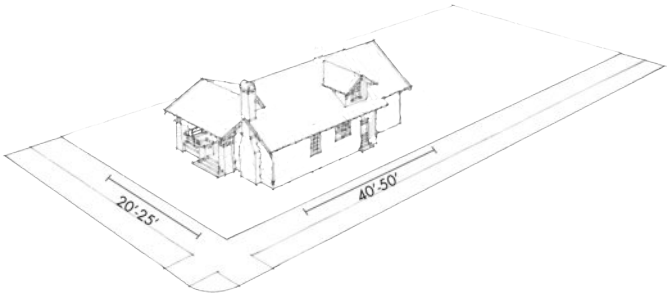
The Two-Story Apartment House is an apt choice for the Neighborhood Center District and Town Center District as a transitional building type of medium to high density. This multi-unit dwelling has the appearance of a large single family unit that can be calibrated in style to match nearby developments.

If located in the TC District, the building shall be set on the property line with a stoop. If the building is located in the NC District, a maximum set back of 10' shall be allowed, and the frontage may be a stoop or a porch.

ONE-STORY

A One-Story Apartment House is comprised of two or three units that share a common wall and separate entrances.

COTTAGE COURT I HOUSE



COTTAGE COURT

The Cottage Court building type consists of a series of small, detached structures located on individual lots, arranged to define a shared court that is typically perpendicular to the street. The shared court takes the place of a private open space and becomes an important community-enhancing element of this type.

This type is appropriately scaled to fit within primarily single-family neighborhoods and is important for providing affordability and a broad choice of housing types that promote walkability.

This type allows for increased density in a detached house format. Homes share a communal yard; this space can either be an edible garden with alloted plots, a playground, or a shared amenity for the residents of the court.

SINGLE-FAMILY HOUSE

A detached Single-Family House is typically 1 1/2 stories. The 1/2 story accommodates bedrooms in a sloping roof. Full vertical walls are therefore only seen on one story, at least on the front and rear elevations.

The frontage of the building may be on the property line or at a maximum setback of 20'. These houses may be enclosed in a low fence.

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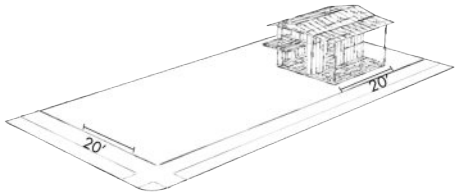
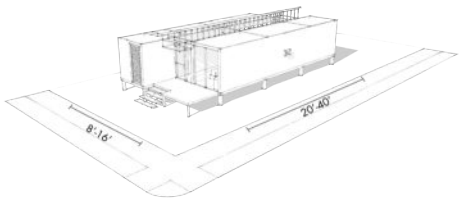




PRIMARY RESIDENTIAL UNIT

Micro Primary Residential Units function as semipermanent to permanent building types. Materials and styles vary from traditional to modern while maintaining affordability. They are single family units, typically 1 to 2 stories high.

They are a versatile building type that can function as main structures or as accessory units. Larger lots in the Neighborhood Center District may be subdivided into micro lots in which the cottage court District and building standards apply.

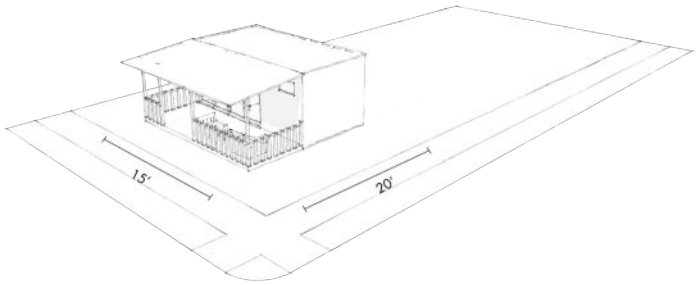
ACCESSORY RESIDENTIAL UNIT

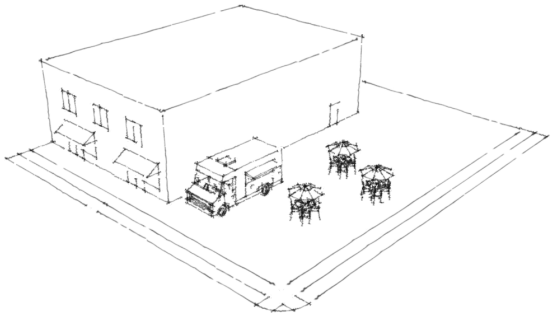
Accessory Residential Units are present across the Neighborhood Center and Neighborhood Districts, and can either be attached or detached. Locations are described in the District Standards. Although a specific design is not prescribed, these units are typically garages, and shall, at a minimum, present elements of design like windows and an access door separate from the vehicle access door. The amount of fenestration in a garage must be a minimum of 30% of the total structure. In general, the size of the accessory unit shall be no larger than 25% of the total mass of the main structure.

CONTAINER HOME

Containers that were originally and currently used for ocean, rail, and road shipment of goods are now being repurposed for residential occupancy (and retail store fronts). Ponderay's adjacency to the rail road can be seen as a unique trait to be highlighted through architecture. Container Homes are affordable and durable. Designs may range from traditional to slightly modern, depending on preference. Typical sizes for container homes vary from 20' to 40' for individual units. However, Container Homes may be composed of 1 or more containers of various sizes.

MICRO RETAIL





*Dimensions vary

GENERAL MICRO RETAIL

The General Micro Retail building type serves multiple functions. It can be calibrated to the Town Center and Neighborhood Center Districts. Micro Retail may be used as a transitional building type, and can also be considered a permanent building type. It allows for rapid and cost-effective reconfiguration of spaces. Uses typically vary from services like coffee shops, barbershops, and fish stands to small restaurants and shops.

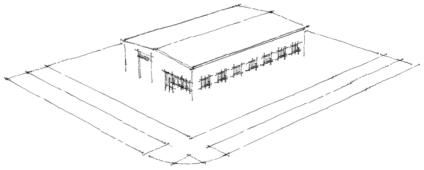
This building type allows for high density and a wide variety of uses within a lot or area depending on configuration. One of the most common materials used is the retrofitted shipping container. In Ponderay, the container shops may serve as a modern twist on the railroad-tied heritage of the town.

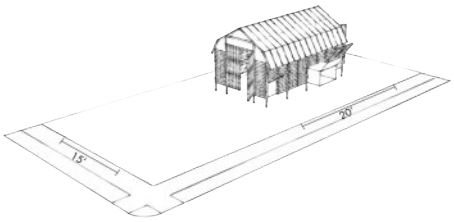
FOOD TRUCKS

Food Trucks are mobile dining vehicles that can be used to activate underutilized spaces, like large parking lots and/or vacant lots. The typical food truck measures about 20 feet long and 8 feet wide, similar to a small Container building type. Food Trucks are entirely temporary/transitional, but can encourage the development of empty areas into spaces with public amenities.

Cities have different ways of regulating Food Trucks. Vendor permits are almost always required for their function.

INDUSTRIAL BUILDING





INDUSTRIAL

Industrial Buildings are large footprint buildings which house industrial uses and are found in the Special District (Industrial) Zone. This building type may be faced with a variety of materials, from traditional concrete and brick to metal siding. Industrial Buildings may have an active first story, but shall not be bound by the frontage requirements of the code. Building footprint dimensions and heights shall be as detailed on the Special District Zone page of this code (Page X-X).

MICRO INDUSTRIAL/GARAGES

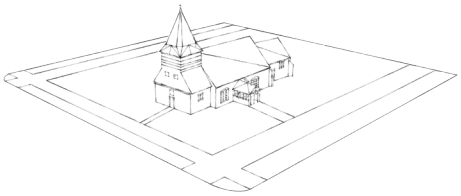
Micro Industrial structures are a type of accessory unit present across the Special and Neighborhood Center Districts. The heights, as well as the overall scape of these structures, varies depending on the size of the vehicles stored. The structure shall have ample lighting.

Although a specific design is not prescribed, commercial vehicle storage units shall, at a minimum, present elements of design like windows and an access door separate from the vehicle access door in order to enhance visual transparency between the public realm and the parking structure. The amount of fenestration in this type of structure must be a minimum of 30%. The images on this page refer to large-scale structures that may be adapted to fit the size constraints of residential zones.

PUBLIC BUILDINGS

Ponderay's public buildings are called out in the map below.

Figure 24: Ponderay Public Buildings.



*Dimensions vary

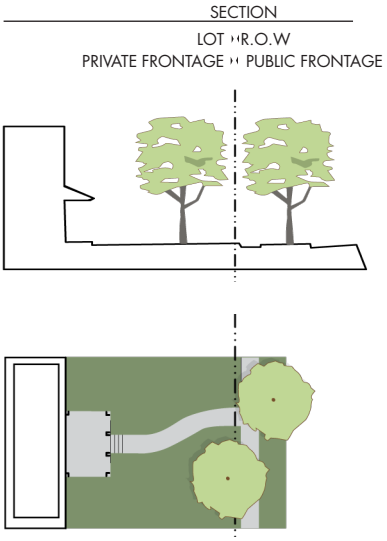
CIVIC BUILDING

A Civic Building is a detached building designed to stand apart from its surroundings due to public or semi-public function for public activity Examples of civic buildings include libraries, churches, court houses, schools, centers of government, performing arts, and museums. They are often the most prominently sited and architecturally significant structures in a community.

BUILDING FRONTS

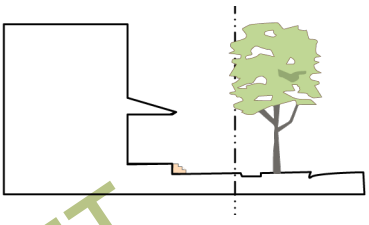
A. Common Yard: A planted frontage wherein the facade is set back substantially from the frontage line. The front yard created remains unfenced and is visually continuous with adjacent yards, supporting a common landscape. The deep setback provides a buffer from the higher speed thoroughfares.

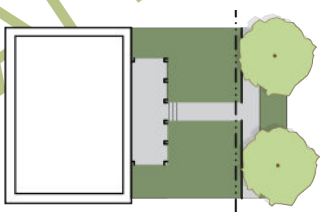
NC N



B. Porch: A planted frontage wherein the facade is set back from the frontage line with an attached porch permitted to encroach. A fence at the frontage line maintains street spatial definition. Porches shall be no less than 8 feet deep.

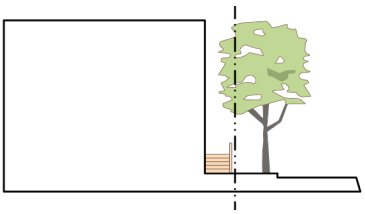
NC N

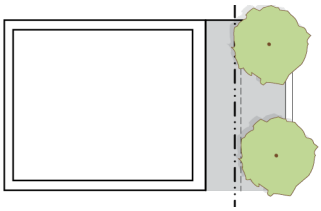




C. Stoop: A frontage wherein the facade is aligned close to the frontage line with the first story elevated from the sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing. This type is recommended for ground-floor residential use.

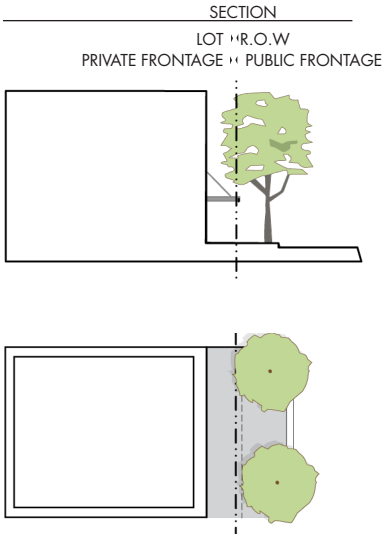
TC NC N





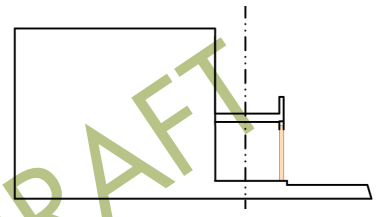
D. Shopfront: A frontage wherein the facade is aligned close to the frontage line with the building entrance at sidewalk grade. This type is conventional for retail use. It has a substantial glazing on the sidewalk level and an awning that may overlap the sidewalk to within 2 feet of the curb. Syn: Retail Frontage.

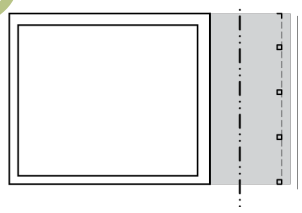
TC TCL NC



E. Gallery: A frontage wherein the facade is aligned close to the frontage line with an attached cantilevered shed or a lightweight colonnade overlapping the sidewalk. The gallery shall be no less than 10 feet wide and should overlap the sidewalk to within 2 feet of the curb. A similar type of frontage with usable space above the sidewalk, known as an Arcade, is also permitted.

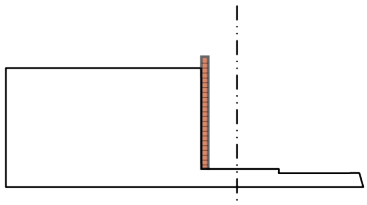
TC TCL

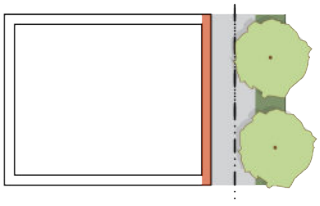




F. Facade: The face of a building that faces the Principal Front. Facades can also be added onto principal structures to transition a building from a temporary to permanent and public-facing condition, as in the case of the Quonset Hut Main Street Building type.

TC TCL NC



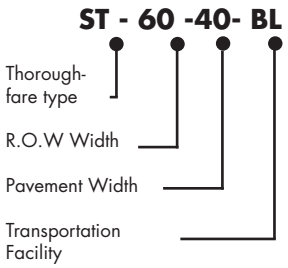


Section 3:
Street Standards

STREET ATLAS

These 11 thoroughfare assemblies are meant to be a general guide for the street hierarchy of Ponderay. Each thoroughfare type has been assigned a color, and its location within the existing street network is indicated in the map below.

The key (right) gives the thoroughfare type followed by the right-of-way width, pavement width, and specialized transportation capabilities where provided.

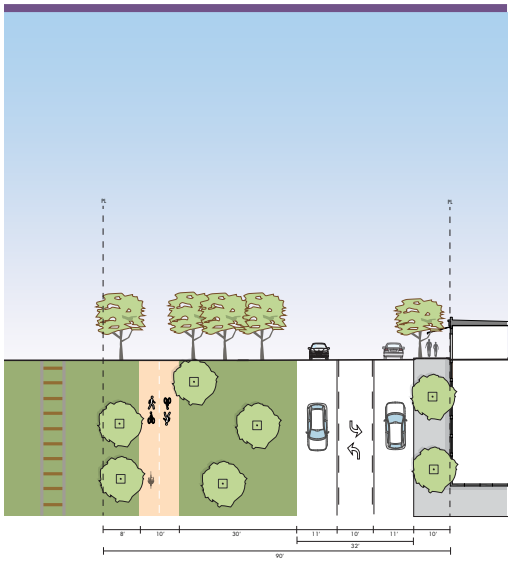


KEY CODES

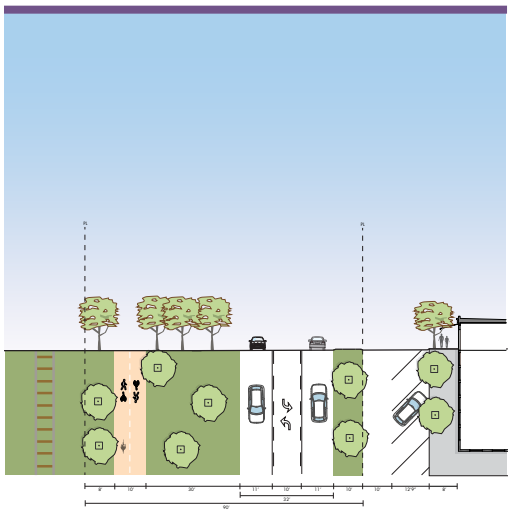
Highway :	HW
Avenue:	AV
Drive:	DR
Urban Street:	US
Neighborhood Street:	NS
Shared Street:	SA
Alley:	A
Shared Use Path:	SP
Bike Lane:	BL
Bike Route:	BR

Figure 25: Proposed Street atlas.

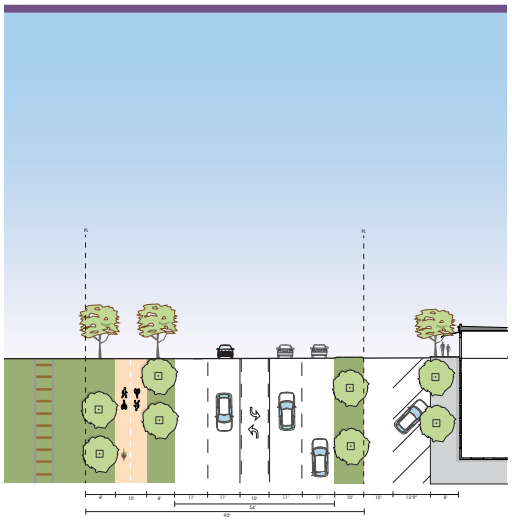
	HW-90-32-SP
Thoroughfare Type	Highway
Transect Zone	TC, TCL
Right of Way Width	90 feet
Pavement Width	32 feet
Movement	Free Movement
Design Speed	50 mph
Travel Lanes	2 Lanes
Parking Lanes	None
Curb	Raised Curb
Walkway Type	8 - 10 Foot Sidewalk
Planter Type	4x4 Tree Well
Landscape Type	Trees at 30 feet average
Transportation Facility	Shared Use Path



	HW-90-32-SP
Thoroughfare Type	Highway
Transect Zone	TC, TCL
Right of Way Width	90 feet
Pavement Width	32 feet
Movement	Free Movement
Design Speed	50 mph
Travel Lanes	2 Lanes + Slip Lane
Parking Lanes	None
Curb	Raised Curb
Walkway Type	8 - 10 foot Sidewalk
Planter Type	4x4 Tree Well
Landscape Type	Trees at 30 feet average
Transportation Facility	Shared Use Path

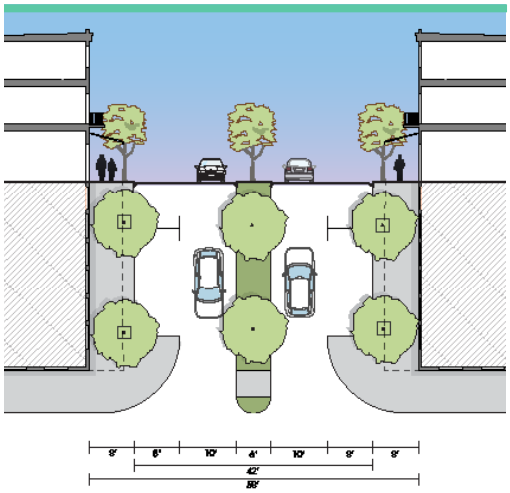


	HW-90-54-SP
Thoroughfare Type	Highway
Transect Zone	TC, TCL
Right of Way Width	90 feet
Pavement Width	54 feet
Movement	Free Movement
Design Speed	50 mph
Travel Lanes	4 Lanes + Slip Lane
Parking Lanes	None
Curb	Raised Curb
Walkway Type	8 -10 foot Sidewalk
Planter Type	4x4 Tree Well
Landscape Type	Trees at 30 feet average
Transportation Facility	Shared Use Path

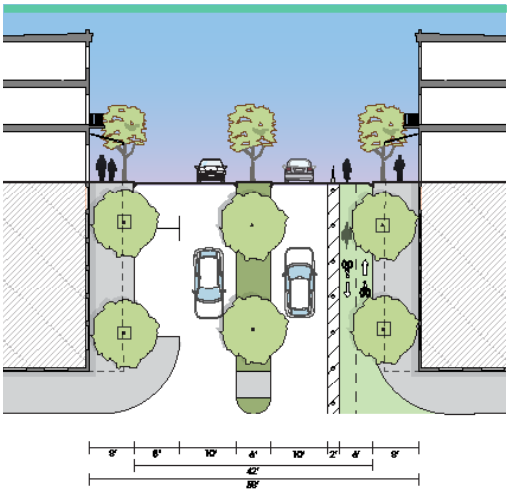


AVENUE | DRIVE

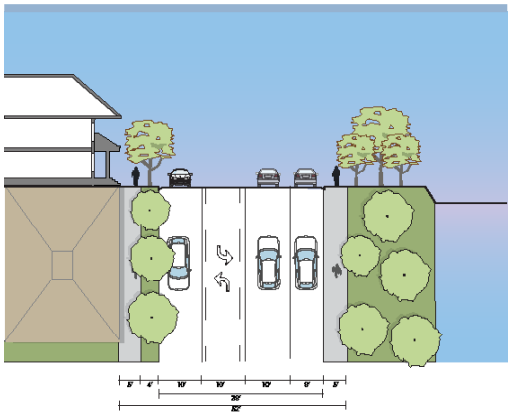
	AV-58-42
Thoroughfare Type	Avenue
Transect Zone	TCL
Right of Way Width	58 feet
Pavement Width	42 feet
Movement	Free Movement
Design Speed	30 mph
Travel Lanes	2 Lanes
Parking Lanes	Parallel Parking x 2
Curb	Raised Curb
Walkway Type	8 -10 foot Sidewalk
Planter Type	4x4 Tree Well
Landscape Type	Trees at 30 feet average
Transportation Facility	None



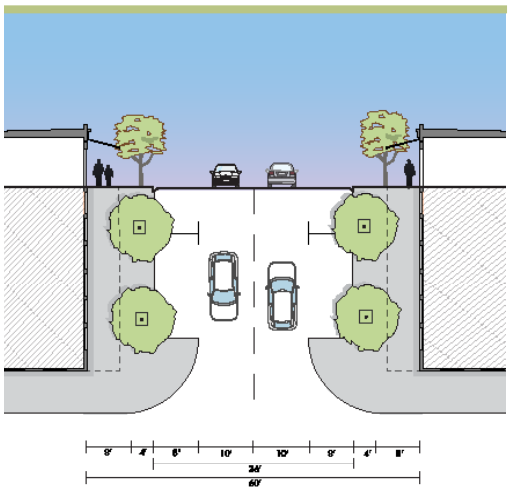
	AV-58-42-BL
Thoroughfare Type	Avenue
Transect Zone	TCL
Right of Way Width	58 feet
Pavement Width	42 feet
Movement	Free Movement
Design Speed	30 mph
Travel Lanes	2 Lanes
Parking Lanes	Parallel Parking x 1
Curb	Raised Curb
Walkway Type	8 -10 foot Sidewalk
Planter Type	4x4 Tree Well
Landscape Type	Trees at 30 feet average
Transportation Facility	2-way sidewalk level protected bike lane



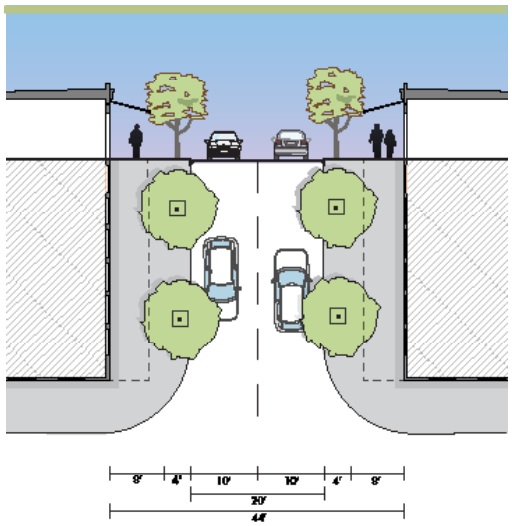
	DR-52-38
Thoroughfare Type	Drive
Transect Zone	TC, NC, N
Right of Way Width	52 feet
Pavement Width	38 feet
Movement	Free Movement
Design Speed	30 mph
Travel Lanes	3 Lanes, 1 Turning Lane
Parking Lanes	Parallel Parking x 1
Curb	Raised Curb
Walkway Type	5 foot Sidewalk
Planter Type	4x4 Tree Well
Landscape Type	Trees at 30 feet average
Transportation Facility	None



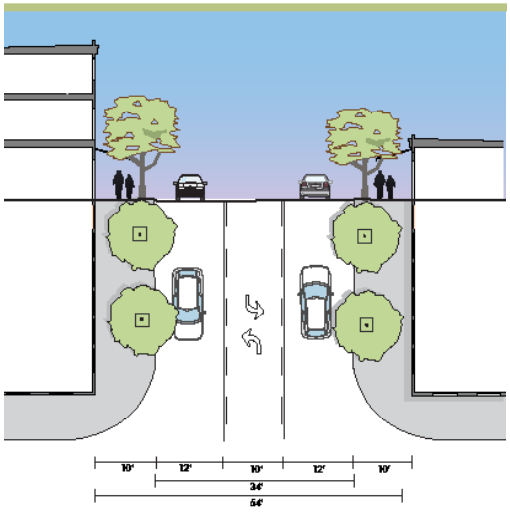
	US-60-36
Thoroughfare Type	Urban Street
Transect Zone	TC
Right of Way Width	60 feet
Pavement Width	36 feet
Movement	Free Movement
Design Speed	30 mph
Travel Lanes	2 Lanes
Parking Lanes	Parallel Parking x 2
Curb	Raised Curb
Walkway Type	8 -10 foot Sidewalk
Planter Type	4x4 Tree Well
Landscape Type	Trees at 30 feet average
Transportation Facility	None



	US-44-20
Thoroughfare Type	Urban Street
Transect Zone	TC
Right of Way Width	44 feet
Pavement Width	20 feet
Movement	Free Movement
Design Speed	30 mph
Travel Lanes	2 Lanes
Parking Lanes	None
Curb	Raised Curb
Walkway Type	8 -10 foot Sidewalk
Planter Type	4x4 Tree Well
Landscape Type	Trees at 30 feet average
Transportation Facility	None

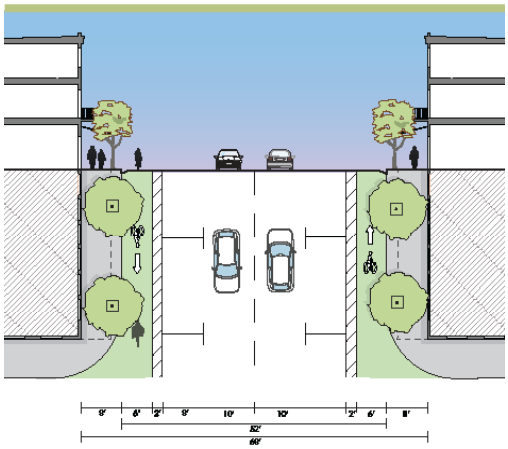


	US-54-34
Thoroughfare Type	Urban Street
Transect Zone	TC, SD
Right of Way Width	54 feet
Pavement Width	34 feet
Movement	Free Movement
Design Speed	30 mph
Travel Lanes	2 Lanes
Parking Lanes	None
Curb	Raised Curb
Walkway Type	8 -10 foot Sidewalk
Planter Type	4x4 Tree Well
Landscape Type	Trees at 30 feet average
Transportation Facility	None

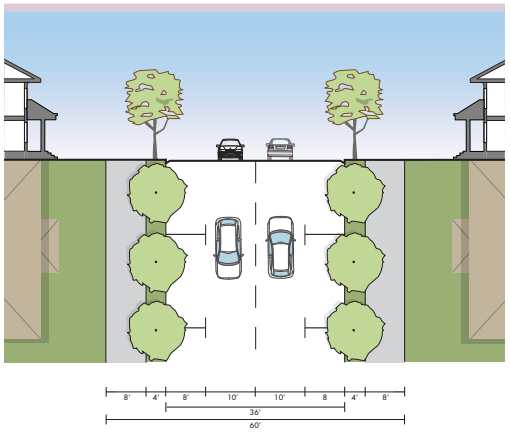


URBAN STREET | NEIGHBORHOOD STREET

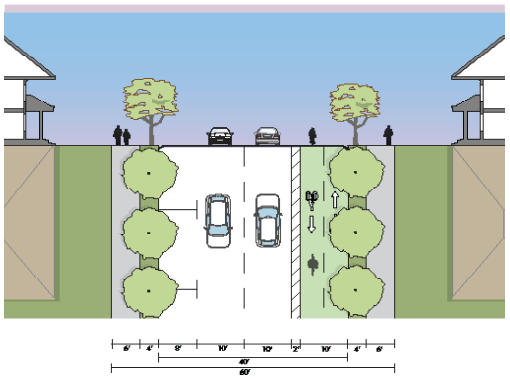
	US-68-52-BL
Thoroughfare Type	Urban Street
Transect Zone	NC
Right of Way Width	68 feet
Pavement Width	52 feet
Movement	Free Movement
Design Speed	25 mph
Travel Lanes	2 Lanes
Parking Lanes	Parallel Parking x 2
Curb	Raised Curb
Walkway Type	8 -10 foot Sidewalk
Planter Type	4x4 Tree Well
Landscape Type	Trees at 30 feet average
Transportation Facility	1-way street level protected bike lanes on each side



	NS-60-36
Thoroughfare Type	Neighborhood Street
Transect Zone	N, NC
Right of Way Width	60 feet
Pavement Width	36 feet
Movement	Free Movement
Design Speed	30 mph
Travel Lanes	Parallel Parking x 2
Parking Lanes	Two Lanes
Curb	Raised Curb
Walkway Type	5-8 foot Sidewalk
Planter Type	4x4 Tree Well
Landscape Type	Trees at 30 feet average
Transportation Facility	None

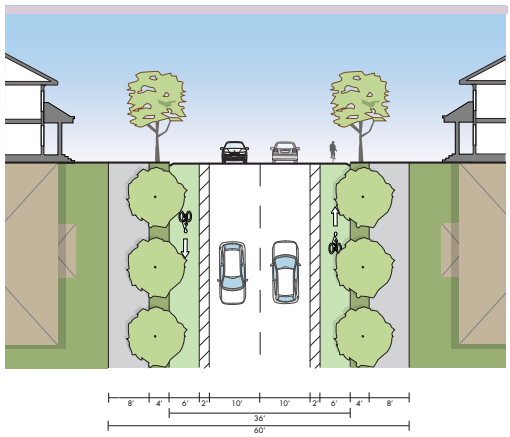


	NS-60-40-BL
Thoroughfare Type	Neighborhood Street
Transect Zone	N, NC
Right of Way Width	60 feet
Pavement Width	40 feet
Movement	Free Movement
Design Speed	25 mph
Travel Lanes	2 Lanes
Parking Lanes	Parallel Parking x 1
Curb	Raised Curb
Walkway Type	5-8 foot sidewalk
Planter Type	4x4 Tree Well
Landscape Type	Trees at 30 feet average
Transportation Facility	2-way sidewalk level protected bike lane

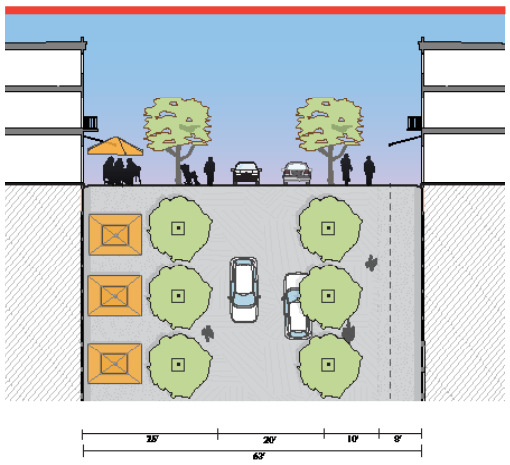


NEIGHBORHOOD STREET | SHARED STREET | REAR ALLEY

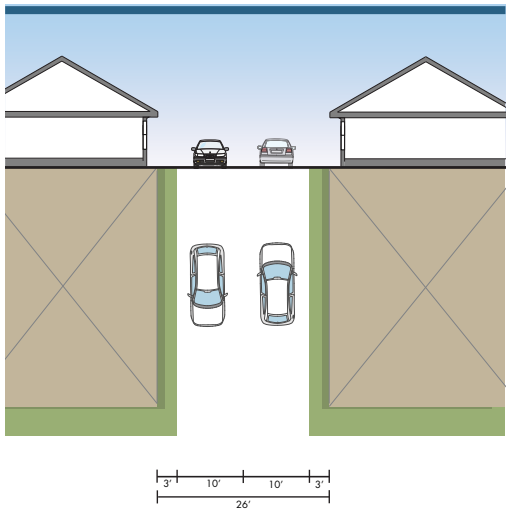
	NS-60-36-BL
Thoroughfare Type	Neighborhood Street
Transect Zone	N, NC
Right of Way Width	60 feet
Pavement Width	36 feet
Movement	Free Movement
Design Speed	25 mph
Travel Lanes	2 Lanes
Parking Lanes	None
Curb	Raised Curb
Walkway Type	5-8 foot sidewalk
Planter Type	4x4 Tree Well
Landscape Type	Trees at 30 feet average
Transportation Facility	1-way street level protected bike lanes on each side



	SS-57-34-BL
Thoroughfare Type	Shared Street
Transect Zone	TC
Right of Way Width	57 feet
Pavement Width	34 feet
Movement	Free Movement
Design Speed	20 mph
Travel Lanes	2 Lanes Unmarked
Parking Lanes	None
Curb	None
Walkway Type	8 -10 foot pedestrian only
Planter Type	4x4 Tree Well
Landscape Type	Trees at 30 feet average
Transportation Facility	None

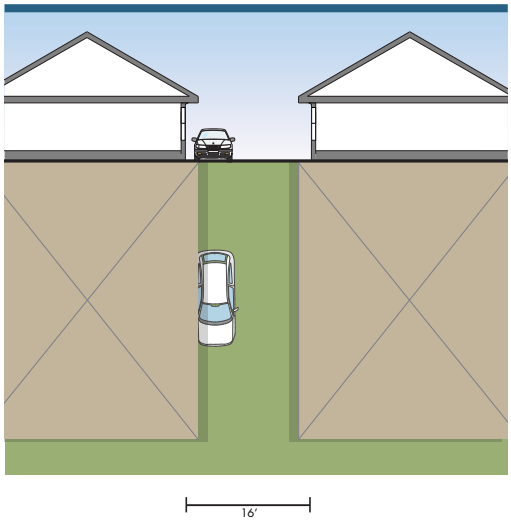


	RA-26-20
Thoroughfare Type	Rear Alley
Transect Zone	TC, SD, N, NC
Right of Way Width	26 feet
Pavement Width	20 feet
Movement	Free Movement
Design Speed	20 mph
Travel Lanes	2 Lanes Unmarked
Parking Lanes	None
Curb	Raised Curb/Swale
Walkway Type	None
Planter Type	4x4 Tree Well
Landscape Type	Trees at 30 feet average
Transportation Facility	None



REAR ALLEY

	RA-16
Thoroughfare Type	Rear Alley
Transect Zone	TC, SD, N, NC
Right of Way Width	16 feet
Pavement Width	0 feet
Movement	Free Movement
Design Speed	20 mph
Travel Lanes	1 Lane
Parking Lanes	None
Curb	None
Walkway Type	None
Planter Type	4x4 Tree Well
Landscape Type	Trees at 30 feet average
Transportation Facility	None

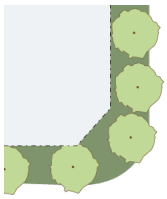
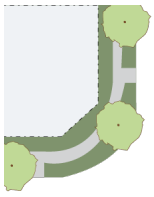
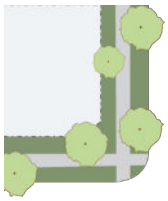
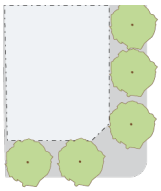
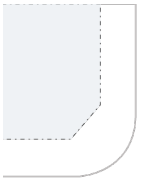
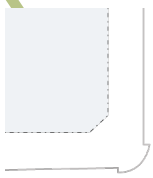
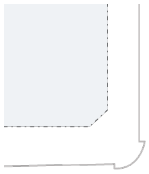
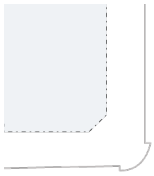
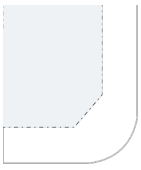
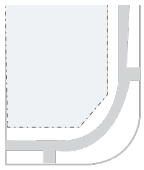
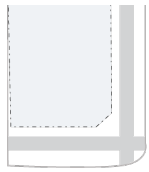
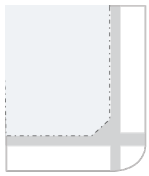
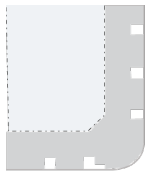
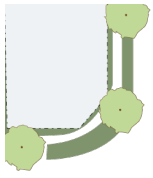
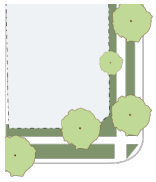
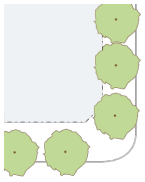


STREETSCAPE: SIDEWALKS AND CURBS

The below standards outline the four primary elements of a streetscape: Assembly, Curb, Walkway, and Planter. These elements comprise the final details in constructing a new street, together with the configurations presented in the previous Sections. They are the elements that determine the pedestrian experience, and how buildings interact with the street.

The appropriate street, or thoroughfare, type and District guide the location of each element's sub-type.

Highway :	**HW**	Neighborhood Street:	**NS**
Avenue:	**AV**	Shared Street:	**SA**
Urban Street:	**US**	Rear Alley:	**RA**
Drive	**DR**	Bike Lane:	**BL**
		Bike Route:	**BR**

STREET TYPE | **DISTRICT**	HW,DR,NS | **TC,N,OS**	DR,NS | **N**	AV,US,DR,NS | **N**	AV,US,DR,NS | **N,NC,TCL**	AV,US,SS | **N,TC,TCL,NC,**
A. ASSEMBLY: The principle variables are the type and dimension of curbs, walkways, planters and landscapes.					
TOTAL WIDTH	8 - 24 FEET	8- 12 FEET	8- 16 FEET	9- 16 FEET	8 - 16 FEET
B. CURB: The detailing of the edge of vehicular pavement, incorporating drainage.					
TYPE	OPEN SWALE	OPEN SWALE	RAISED CURB	RAISED CURB	RAISED CURB
RADIUS	10-30 FEET	10-30 FEET	5-20 FEET	4-8 FEET	5 - 20 FEET
C. WALKWAY: The portion of the thoroughfare dedicated exclusively to pedestrian activity.					
TYPE	PATH OPTIONAL	PATH	SIDEWALK	SIDEWALK	SIDEWALK
WIDTH	N/A	4-8 FEET	4-8 FEET	4-8 FEET	12-30
D. PLANTER: The portion of the thoroughfare accommodating street trees and other landscape					
ARRANGEMENT	CLUSTERED	CLUSTERED	REGULAR	REGULAR	OPPORTUNISTIC
SPECIES	MULTIPLE	MULTIPLE	ALTERNATING	ALTERNATING	SINGLE
PLANTER TYPE	SWALE	SWALE	PLANTING STRIP	PLANTING STRIP	TREE WELL
PLANTER WIDTH	8-16 FEET	8-16 FEET	4-8 FEET	4-8 FEET	4-6 FEET